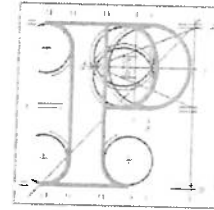


Our Case Number: ABP-320300-24



An
Bord
Pleanála

Transport Infrastructure Ireland
Land Use Planning
Parkgate Business Centre
Parkgate Street
Dublin 8
D08 DK10

Date: 09 September 2024

Re: Proposed development of a Gas Insulated Switchgear (GIS) substation compound
In the townlands of Carhoona, Carhoonakilla, Carhoonakineely, Cockhill, Coolnagoonagh,
Farranawana, Kilcolgan Lower, Kilcolgan Upper, Kilpaddoge and Ralappane, County Kerry
(www.STEP220kVConnection.com)

Dear Sir / Madam,

An Bord Pleanála has received your submission in relation to the above mentioned proposed development and will take it into consideration in its determination of the matter.

The Board will revert to you in due course in respect of this matter.

Please be advised that copies of all submissions / observations received in relation to the application will be made available for public inspection at the offices of the local authority and at the offices of An Bord Pleanála when they have been processed by the Board.

More detailed information in relation to strategic infrastructure development can be viewed on the Board's website: www.pleanala.ie.

If you have any queries in the meantime, please contact the undersigned officer of the Board or email sids@pleanala.ie quoting the above mentioned An Bord Pleanála reference number in any correspondence with the Board.

Yours faithfully,

Ellen Moss
Ellen Moss
Executive Officer
Direct Line: 01-8737285

VA11

Tel	Tel	(01) 858 8100
Glaao Áitiúil	LoCall	1800 275 175
Facs	Fax	(01) 872 2684
Láithreán Gréasáin	Website	www.pleanala.ie
Ríomhphost	Email	bord@pleanala.ie

64 Sráid Maoilbhríde	64 Marlborough Street
Baile Átha Cliath 1	Dublin 1
D01 V902	D01 V902

Ellen Moss

From: Landuse Planning <LandUsePlanning@tii.ie>
Sent: 05 September 2024 14:08
To: SIDS
Subject: ABP Case ref. VA08.320300
Attachments: ABP Case ref. VA08.320300.pdf
Categories: Ellen

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Dear Sir/Madam,

Please find attached a copy of TII observations in relation to the above application.

Yours sincerely,
Michael McCormack
Senior Land Use Planner

In accordance with TII's Right to Disconnect policy, if you are receiving this email outside of normal working hours, I do not expect a response or action outside of your own working hours unless it is clearly noted as requiring urgent attention.

De réir pholasáí BIÉ An Ceart gan a bheith Ceangailte, má tá an ríomhphost seo á fháil agat lasmuigh de na gnáthuaireanta oibre, nílím ag súil le freagra ná le gníomh uait lasmuigh de do ghnáthuaireanta oibre féin mura bhfuil sé ráite go soiléir go bhfuil gá gníomhú go práinneach.

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The Secretary
An Bord Pleanála
64 Marlborough St.
Dublin 1
D01 V902

by e.mail; sids@pleanala.ie

Dáta | Date
5 September, 2024

Ár dTag | Our Ref.
TII24-128229

Re. Strategic Infrastructure Development – Proposed development of a Gas Insulated Switchgear (GIS) substation compound at Carhoona, Carhoonakilla, Carhoonakineely, Cockhill, Coolnagoonagh, Farranawana, Kilcolgan Lower, Kilcolgan Upper, Kilpaddoge and Ralappane, County Kerry

(ABP Case ref. VA08.320300)

Dear Secretary,

The Authority acknowledges receipt of referral of the above proposed Electricity Development Application on behalf of Shannon LNG Limited. It is proposed to address the proposed development in relation to the provisions of official policy and in relation to national road network maintenance and safety.

1. Official Policy

The Board will be aware that official policy concerning development management and access to national roads is outlined in the Section 28 Ministerial Guidelines 'Spatial Planning and National Roads Guidelines for Planning Authorities' (DoECLG, 2012).

Section 2.5 of the DoECLG Guidelines sets out policy that seeks to avoid the creation of additional access points from new development or the generation of increased traffic from existing accesses (i.e. non-public road access) to national roads, to which speed limits greater than 50 kph apply.

In that regard, the Authority acknowledges that access to the development location is facilitated via the local road network in the first instance prior to access to the N67 and N69, national roads.

2. National Road Network Maintenance and Safety

In addition to the above, there are a number of operational issues related to the subject development proposal, in the Authority's opinion, that are required to be considered to address network maintenance and road safety prior to any decision on this planning application.

2.1 Proposed Development Haul Route

Chapter 17 of the EIAR assesses the potential roads and traffic impacts associated with the proposed development. It is noted that the EIAR does not identify any works to the national road network to facilitate construction or delivery of development components to site.

In the interests of clarity, TII advises that any works to the national road network to facilitate delivery of abnormal loads (length, width, height or weight) to site shall comply with TII Publications and shall be subject to Road Safety

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TII processes personal data in accordance with its Data Protection Notice available at www.tii.ie.



Bonneagar Iompair Éireann
Ionad Gnó Gheata na Páirce
Sráid Gheata na Páirce
Baile Átha Cliath 8
Éire, D08 DK10



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Audit as appropriate. Subject to the outcome of Road Safety Audit, works should ensure the ongoing safety for all road users.

TII requests referral of all proposals agreed between the road authority and the applicant impacting on national roads. Any mitigation measures identified by the applicant should be included as conditions in any decision to grant permission. Any damage caused to the pavement of the existing national road due to the turning movement of abnormal 'length' loads (eg. tearing of the surface course) shall be rectified in accordance with TII Pavement Standards and details in this regard shall be agreed with the Road Authority prior to the commencement of any development on site.

2.2 Structures

It is noted that Section 17.5.1.7 of the EIAR indicates that there will be no Abnormal Loads associated with the proposed development. Notwithstanding, in the event that project details alter, it should be noted that any operator who wants to transport a vehicle or load whose weight falls outside the limits allowed by the Road Traffic (Construction Equipment & Use of Vehicles) Regulations 2003, SI 5 of 2003, must obtain a permit for its movement from each Local Authority through whose jurisdiction the vehicle shall travel.

With specific reference to national road structures on the proposed haul route, all structures should be checked by the applicant/developer to confirm that all the structures can accommodate the proposed loading associated with the delivery of development components to site where the weight of the delivery vehicle and load exceeds that permissible under the Road Traffic Regulations.

In addition, although not indicated as a feature of this application and development proposal, while an abnormal load is defined as anything above 46 tonnes and below 200 tonnes, any load above that, represents an 'Exceptional Abnormal Load' ('EAL'). All structures to be crossed will need a full structural assessment by the developer in accordance with TII Publications AM-STR-06048 to verify that they can sustain any 'EAL' load safely and without any damage.

Full details of the transportation of all Abnormal Loads and all 'Exceptional Abnormal Loads' associated with the subject development shall be agreed with all planning and road authorities along all proposed haul routes prior to the commencement of any development.

2.3 Cabling/Trenching

TII notes proposals relating to grid connection and that no new grid connection proposals included in the subject application impact the strategic national road network.

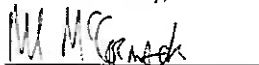
2.4 Greenways

In relation to any Greenway or Active Travel proposals in the vicinity of the proposed works, consultation with Kerry County Councils own internal project and/or design staff is recommended.

Conclusion

It is requested that the above matters are taken into consideration prior to any decision on the subject application.

Yours sincerely,



Michael McCormack
Senior Land Use Planner